



**TRANSPORTATION CABINET**

Frankfort, Kentucky 40622  
www.transportation.ky.gov/

**Steven L. Beshear**  
Governor

**Michael W. Hancock, P.E.**  
Secretary

December 2, 2015

CALL NO. 318  
CONTRACT ID NO. 151091  
ADDENDUM # 1

Subject: Hopkins County, FD04 SPP 054 NEW ROUTE  
Letting December 11, 2015

(1) Revised - Plan Sheets

Proposal revisions are available at <http://transportation.ky.gov/Construction-Procurement/>.

Plan revisions are available at <http://www.lynnimaging.com/kytransportation/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in cursive script that reads "Rachel Mills".

Rachel Mills, P.E.  
Director  
Division of Construction Procurement

RM:ks  
Enclosures

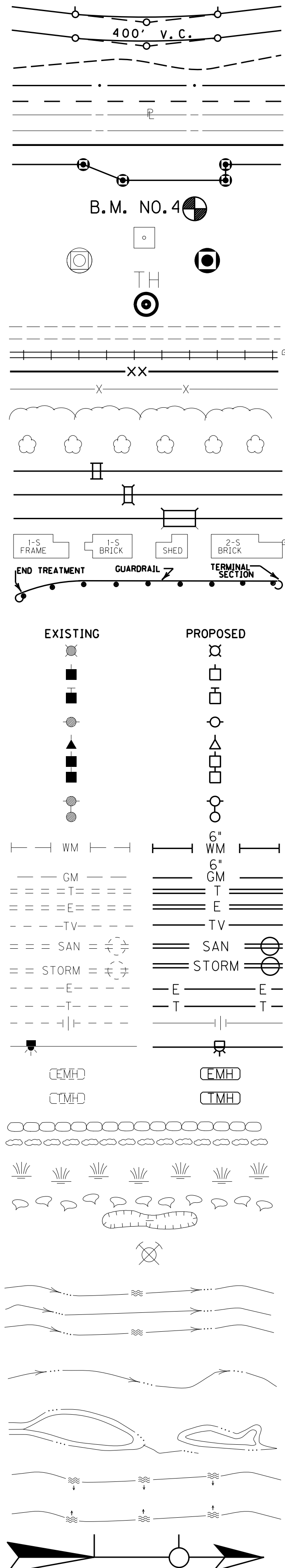


An Equal Opportunity Employer M/F/D

FILE NAME: Y:\KYTC\2014\214-376 SWRDWY HOPKINS CO.2-804-11\DELIVERABLES\SENT TO TRANSPORTATION CABINET\REV REVISIONS\2-804,10 ROW REV #112315  
 USER: swagner  
 DATE PLOTTED: October 31, 2015  
 E-SHEET NAME: MicroStation v8.11.7.443

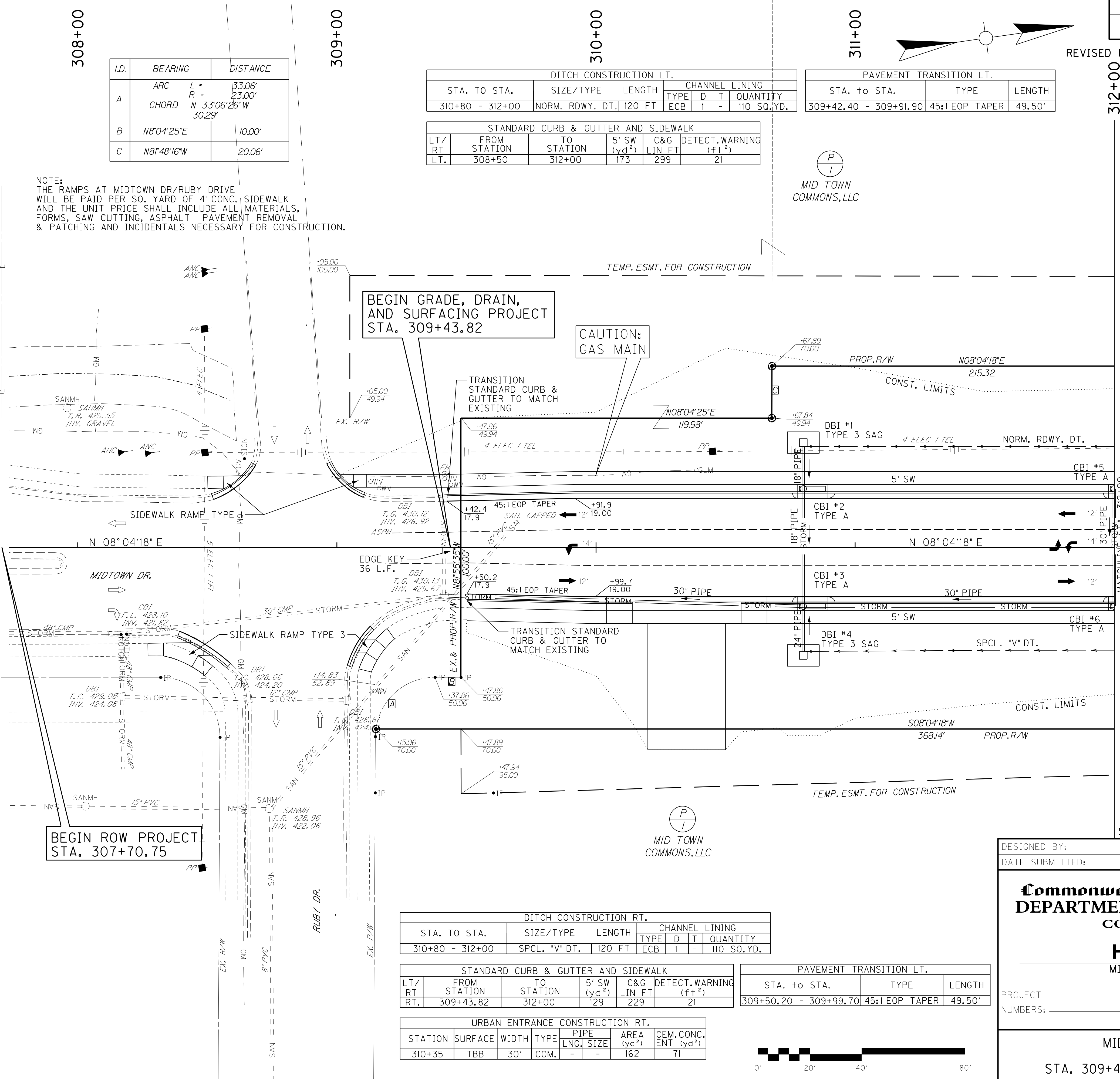
### CONVENTIONAL SIGNS

- SURVEY LINE
- GRADE LINE
- GROUND LINE
- COUNTY LINE
- CORPORATE LIMITS
- EXIST. PROPERTY LINE
- EXIST. RIGHT OF WAY & PROPERTY LINE
- PROPOSED RIGHT OF WAY
- RIGHT OF WAY MONUMENT
- BENCH MARK
- EXISTING R/W MARKER
- RIGHT OF WAY MONUMENT EXISTING/PROPOSED
- UTILITY TEST HOLE
- EXISTING ROAD
- RAILROAD
- FENCE (CONTROLLED ACCESS)
- FENCE (EXCEPT STONE AND HEDGE)
- TREE LINE
- TREES
- PIPE CULVERT
- CULVERT
- BRIDGE
- BUILDINGS
- GUARDRAIL
- LIGHTING POLE
- POWER POLE
- JOINT POWER & TELEPHONE POLE
- TELEPHONE & TELEGRAPH POLE
- ANCHOR, POWER OR TELEPHONE
- STUB POWER
- STUB TELEPHONE
- WATER MAIN
- GAS MAIN
- TELEPHONE DUCT
- ELECTRIC DUCT
- DIRECT BURIAL TV CABLE
- SANITARY SEWER (WITH MANHOLE)
- STORM SEWER (WITH MANHOLE)
- DIRECT BURIAL ELECTRIC CABLE
- DIRECT BURIAL TELEPHONE CABLE
- OVERHEAD WIRE
- TRAFFIC LIGHTS
- ELECTRIC MANHOLE
- TELEPHONE MANHOLE
- STONE FENCE
- HEDGE FENCE
- SWAMP OR MARSH
- SPRINGS
- SINKHOLE
- QUARRY SITE
- BLUE LINE STREAM
- INTERMITTENT STREAM OR DITCH
- LAKES OR PONDS
- REGULATED FLOODWAY
- NORTH POINT



| I.D. | BEARING   | DISTANCE |
|------|---|----------|
| A    | ARC<br>L = 33.06'<br>R = 23.00'<br>CHORD N 33°06'26"W<br>30.29' |          |
| B    | N 8°04'25"E   | 10.00'   |
| C    | N 81°48'16"W  | 20.06'   |

NOTE:  
 THE RAMPS AT MIDTOWN DR/RUBY DRIVE WILL BE PAID PER SQ. YARD OF 4" CONC. SIDEWALK AND THE UNIT PRICE SHALL INCLUDE ALL MATERIALS, FORMS, SAW CUTTING, ASPHALT PAVEMENT REMOVAL & PATCHING AND INCIDENTALS NECESSARY FOR CONSTRUCTION.



| DITCH CONSTRUCTION LT. |                 |        |                |                |
|------------------------|-----------------|--------|----------------|----------------|
| STA. TO STA.           | SIZE/TYPE       | LENGTH | CHANNEL LINING |                |
|                        |                 |        | TYPE           | D T QUANTITY   |
| 310+80 - 312+00        | NORM. RDWY. DT. | 120 FT | ECB            | 1 - 110 SQ.YD. |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |  |
|-------------------------------------|--------------|------------|--------------------------|--|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G DETECT. WARNING (ft <sup>2</sup> ) |
| LT.                                 | 308+50       | 312+00     | 173                      | 299                                    |

| PAVEMENT TRANSITION LT. |                |        |
|-------------------------|----------------|--------|
| STA. TO STA.            | TYPE           | LENGTH |
| 309+42.40 - 309+91.90   | 45:1 EOP TAPER | 49.50' |

BEGIN GRADE, DRAIN, AND SURFACING PROJECT STA. 309+43.82

CAUTION: GAS MAIN

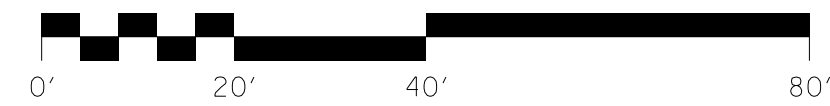
BEGIN ROW PROJECT STA. 307+70.75

| DITCH CONSTRUCTION RT. |               |        |                |                |
|------------------------|---------------|--------|----------------|----------------|
| STA. TO STA.           | SIZE/TYPE     | LENGTH | CHANNEL LINING |                |
|                        |               |        | TYPE           | D T QUANTITY   |
| 310+80 - 312+00        | SPCL. "V" DT. | 120 FT | ECB            | 1 - 110 SQ.YD. |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |  |    |
|-------------------------------------|--------------|------------|--------------------------|--|----|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G DETECT. WARNING (ft <sup>2</sup> ) |    |
| RT.                                 | 309+43.82    | 312+00     | 129                      | 229                                    | 21 |

| URBAN ENTRANCE CONSTRUCTION RT. |         |       |      |                |                         |                                   |
|---------------------------------|---------|-------|------|----------------|-------------------------|-----------------------------------|
| STATION                         | SURFACE | WIDTH | TYPE | PIPE LNG. SIZE | AREA (yd <sup>2</sup> ) | CEM. CONC. ENT (yd <sup>2</sup> ) |
| 310+35                          | TBB     | 30'   | COM. | -              | 162                     | 71                                |

| PAVEMENT TRANSITION LT. |                |        |
|-------------------------|----------------|--------|
| STA. TO STA.            | TYPE           | LENGTH |
| 309+50.20 - 309+99.70   | 45:1 EOP TAPER | 49.50' |



|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R3        |

REVISED PLAN DATE: NOVEMBER 23, 2015

SCALE: 1"=20'

DESIGNED BY: \_\_\_\_\_  
 DATE SUBMITTED: \_\_\_\_\_

**Commonwealth of Kentucky**  
**DEPARTMENT OF HIGHWAYS**  
 COUNTY OF  
**HOPKINS**  
 MIDTOWN DRIVE

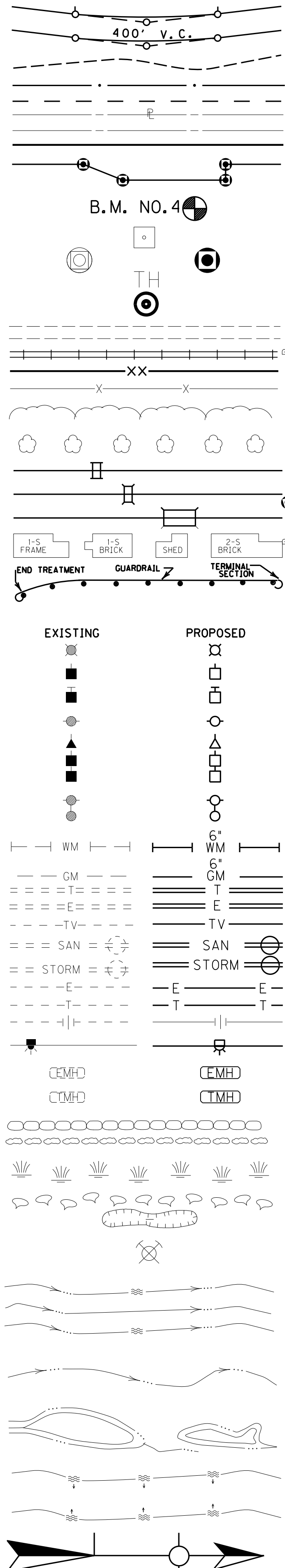
PROJECT \_\_\_\_\_  
 NUMBERS: \_\_\_\_\_

MIDTOWN DRIVE  
 PLAN  
 STA. 309+43.82 TO STA. 312+00



### CONVENTIONAL SIGNS

- SURVEY LINE
- GRADE LINE
- GROUND LINE
- COUNTY LINE
- CORPORATE LIMITS
- EXIST. PROPERTY LINE
- EXIST. RIGHT OF WAY & PROPERTY LINE
- PROPOSED RIGHT OF WAY
- RIGHT OF WAY MONUMENT
- BENCH MARK
- EXISTING R/W MARKER
- RIGHT OF WAY MONUMENT EXISTING/PROPOSED
- UTILITY TEST HOLE
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- FENCE (CONTROLLED ACCESS)
- FENCE (EXCEPT STONE AND HEDGE)
- TREE LINE
- TREES
- PIPE CULVERT
- CULVERT
- BRIDGE
- BUILDINGS
- GUARDRAIL
- LIGHTING POLE
- POWER POLE
- JOINT POWER & TELEPHONE POLE
- TELEPHONE & TELEGRAPH POLE
- ANCHOR, POWER OR TELEPHONE
- STUB POWER
- STUB TELEPHONE
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- OVERHEAD WIRE
- TRAFFIC LIGHTS
- ELECTRIC MANHOLE
- TELEPHONE MANHOLE
- STONE FENCE
- HEDGE FENCE
- SWAMP OR MARSH
- SPRINGS
- SINKHOLE
- QUARRY SITE
- BLUE LINE STREAM
- INTERMITTENT STREAM OR DITCH
- LAKES OR PONDS
- REGULATED FLOODWAY
- NORTH POINT



| I.D. | BEARING   | DISTANCE |
|------|---|----------|
| A    | ARC<br>L = 33.06'<br>R = 23.00'<br>CHORD N 33°06'26"W<br>30.29' |          |
| B    | N 8°04'25"E   | 10.00'   |
| C    | N 81°58'26"W  | 20.05'   |

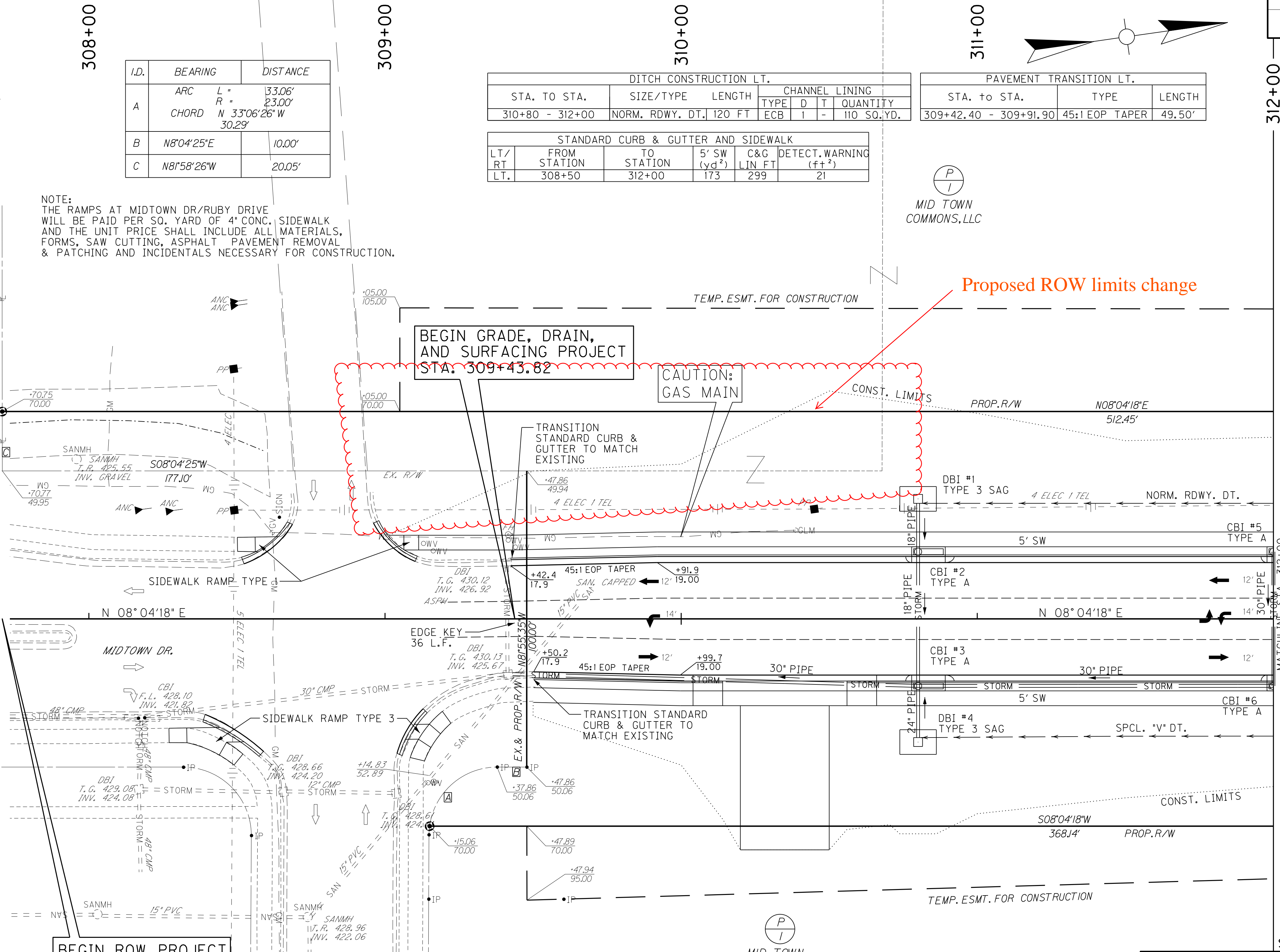
NOTE:  
THE RAMPS AT MIDTOWN DR/RUBY DRIVE WILL BE PAID PER SQ. YARD OF 4" CONC. SIDEWALK AND THE UNIT PRICE SHALL INCLUDE ALL MATERIALS, FORMS, SAW CUTTING, ASPHALT PAVEMENT REMOVAL & PATCHING AND INCIDENTALS NECESSARY FOR CONSTRUCTION.

| DITCH CONSTRUCTION LT. |                 |        |                |   |             |
|------------------------|-----------------|--------|----------------|---|-------------|
| STA. TO STA.           | SIZE/TYPE       | LENGTH | CHANNEL LINING |   |             |
|                        |                 |        | TYPE           | D | T QUANTITY  |
| 310+80 - 312+00        | NORM. RDWY. DT. | 120 FT | ECB            | 1 | 110 SQ. YD. |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| LT.                                 | 308+50       | 312+00     | 173                      | 299        | 21                                 |

| PAVEMENT TRANSITION LT. |                |        |
|-------------------------|----------------|--------|
| STA. TO STA.            | TYPE           | LENGTH |
| 309+42.40 - 309+91.90   | 45:1 EOP TAPER | 49.50' |



BEGIN GRADE, DRAIN, AND SURFACING PROJECT  
STA. 309+43.82

BEGIN ROW PROJECT  
STA. 307+70.75

Proposed ROW limits change

| DITCH CONSTRUCTION RT. |               |        |                |   |             |
|------------------------|---------------|--------|----------------|---|-------------|
| STA. TO STA.           | SIZE/TYPE     | LENGTH | CHANNEL LINING |   |             |
|                        |               |        | TYPE           | D | T QUANTITY  |
| 310+80 - 312+00        | SPCL. "V" DT. | 120 FT | ECB            | 1 | 110 SQ. YD. |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| RT.                                 | 309+43.82    | 312+00     | 129                      | 229        | 21                                 |



DESIGNED BY: \_\_\_\_\_  
DATE SUBMITTED: \_\_\_\_\_

**Commonwealth of Kentucky**  
**DEPARTMENT OF HIGHWAYS**  
COUNTY OF  
**HOPKINS**  
MIDTOWN DRIVE

PROJECT \_\_\_\_\_  
NUMBERS: \_\_\_\_\_

MIDTOWN DRIVE  
PLAN  
STA. 309+43.82 TO STA. 312+00

FILE NAME: Y:\KNTC\2014\214-376 SMDRWY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION CABINET\FINAL CONST PLAN SUBMITTAL 12-804.10  
 USER: swagner  
 DATE PLOTTED: November 5, 2015  
 E-SHEET NAME: MicroStation v8.11.7.443

FILE NAME: Y:\KTYC 2014\214-376 SMDRWY HOPKINS CO.2-804.11\DELIVERABLES\SENT TO TRANSPORTATION CABINET\RW REVISIONS\2-804.10 ROW REV #112315  
 USER: swagner  
 DATE PLOTTED: November 23, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443

|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R5        |

REVISED PLAN DATE: NOVEMBER 23, 2015

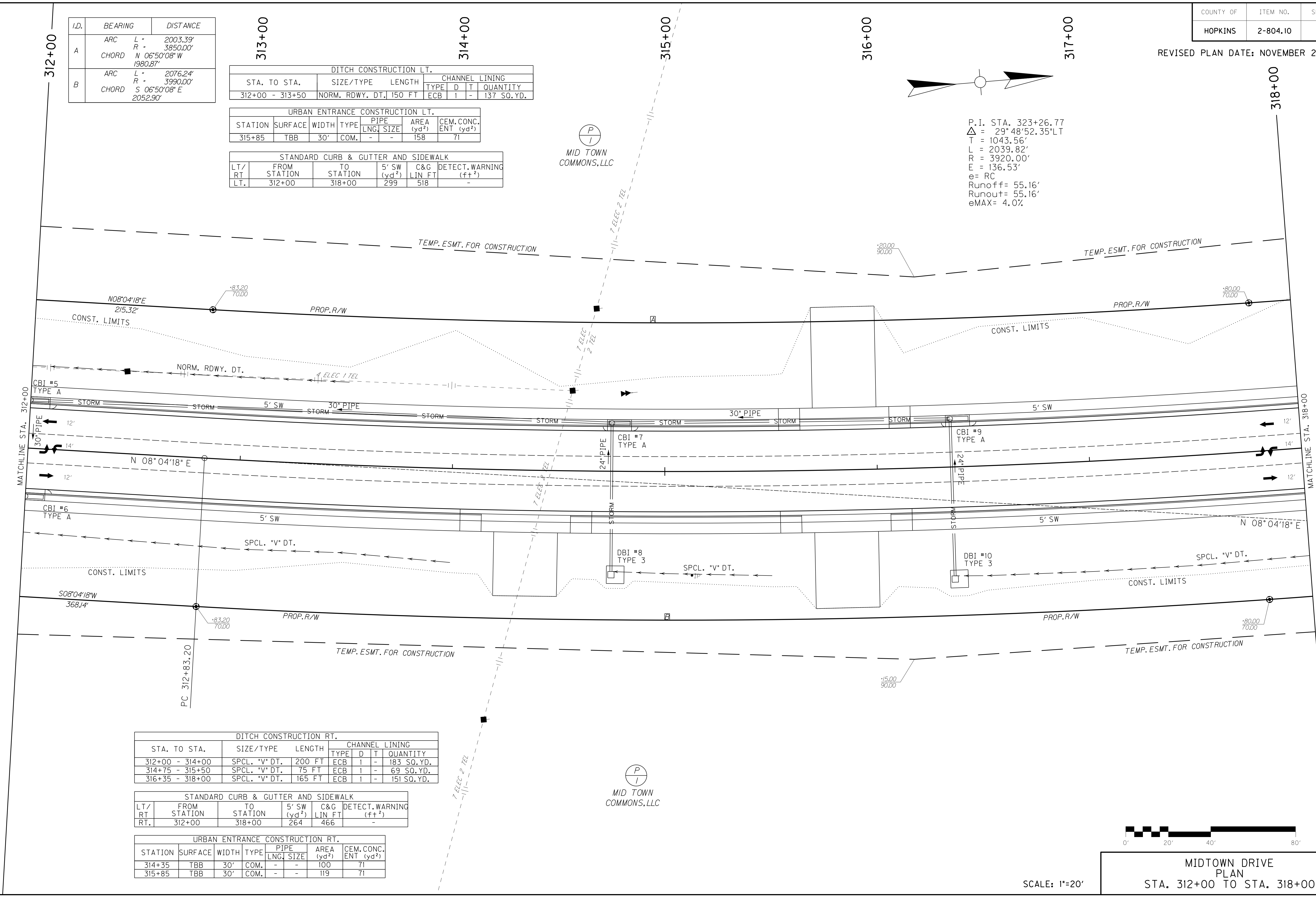
| I.D.                | BEARING          | DISTANCE |
|---------------------|------------------|----------|
| A                   | ARC L = 2003.39' |          |
|                     | R = 3850.00'     |          |
| CHORD N 06°50'08" W |                  | 1980.87' |
| B                   | ARC L = 2076.24' |          |
|                     | R = 3990.00'     |          |
| CHORD S 06°50'08" E |                  | 2052.90' |

| DITCH CONSTRUCTION LT. |                 |        |                |   |   |            |
|------------------------|-----------------|--------|----------------|---|---|------------|
| STA. TO STA.           | SIZE/TYPE       | LENGTH | CHANNEL LINING |   |   |            |
|                        |                 |        | TYPE           | D | T | QUANTITY   |
| 312+00 - 313+50        | NORM. RDWY. DT. | 150 FT | ECB            | 1 | - | 137 SQ.YD. |

| URBAN ENTRANCE CONSTRUCTION LT. |         |       |      |                |                         |                                   |
|---------------------------------|---------|-------|------|----------------|-------------------------|-----------------------------------|
| STATION                         | SURFACE | WIDTH | TYPE | PIPE LNG. SIZE | AREA (yd <sup>2</sup> ) | CEM. CONC. ENT (yd <sup>2</sup> ) |
| 315+85                          | TBB     | 30'   | COM. | -              | 158                     | 71                                |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| LT.                                 | 312+00       | 318+00     | 299                      | 518        | -                                  |

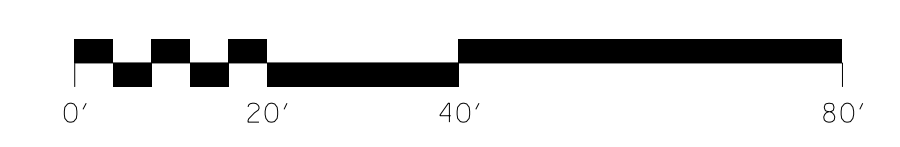
P.I. STA. 323+26.77  
 $\Delta = 29^\circ 48' 52.35" LT$   
 T = 1043.56'  
 L = 2039.82'  
 R = 3920.00'  
 e = 136.53'  
 e = RC  
 Runoff = 55.16'  
 Runout = 55.16'  
 eMAX = 4.0%



| DITCH CONSTRUCTION RT. |               |        |                |   |   |            |
|------------------------|---------------|--------|----------------|---|---|------------|
| STA. TO STA.           | SIZE/TYPE     | LENGTH | CHANNEL LINING |   |   |            |
|                        |               |        | TYPE           | D | T | QUANTITY   |
| 312+00 - 314+00        | SPCL. *V* DT. | 200 FT | ECB            | 1 | - | 183 SQ.YD. |
| 314+75 - 315+50        | SPCL. *V* DT. | 75 FT  | ECB            | 1 | - | 69 SQ.YD.  |
| 316+35 - 318+00        | SPCL. *V* DT. | 165 FT | ECB            | 1 | - | 151 SQ.YD. |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| RT.                                 | 312+00       | 318+00     | 264                      | 466        | -                                  |

| URBAN ENTRANCE CONSTRUCTION RT. |         |       |      |                |                         |                                   |
|---------------------------------|---------|-------|------|----------------|-------------------------|-----------------------------------|
| STATION                         | SURFACE | WIDTH | TYPE | PIPE LNG. SIZE | AREA (yd <sup>2</sup> ) | CEM. CONC. ENT (yd <sup>2</sup> ) |
| 314+35                          | TBB     | 30'   | COM. | -              | 100                     | 71                                |
| 315+85                          | TBB     | 30'   | COM. | -              | 119                     | 71                                |



SCALE: 1"=20'

MIDTOWN DRIVE  
 PLAN  
 STA. 312+00 TO STA. 318+00



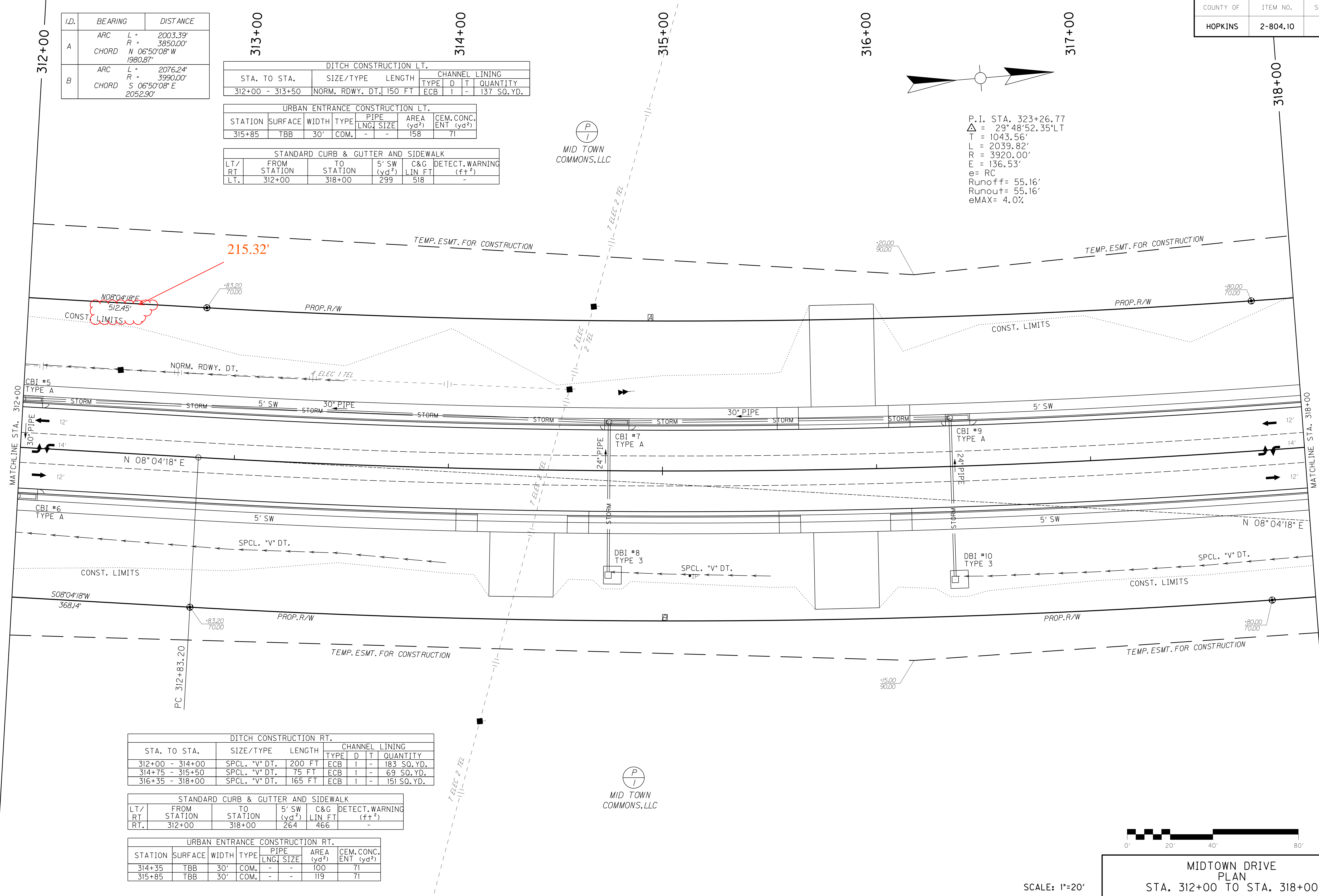
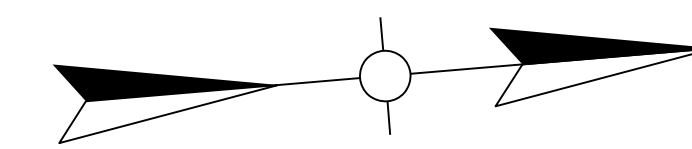
| I.D.                | BEARING          | DISTANCE |
|---------------------|------------------|----------|
| A                   | ARC L = 2003.39' |          |
|                     | R = 3850.00'     |          |
| CHORD N 06°50'08" W |                  | 1980.87' |
| B                   | ARC L = 2076.24' |          |
|                     | R = 3990.00'     |          |
| CHORD S 06°50'08" E |                  | 2052.90' |

| DITCH CONSTRUCTION LT. |                 |        |                |   |   |            |
|------------------------|-----------------|--------|----------------|---|---|------------|
| STA. TO STA.           | SIZE/TYPE       | LENGTH | CHANNEL LINING |   |   |            |
|                        |                 |        | TYPE           | D | T | QUANTITY   |
| 312+00 - 313+50        | NORM. RDWY. DT. | 150 FT | ECB            | 1 | - | 137 SQ.YD. |

| URBAN ENTRANCE CONSTRUCTION LT. |         |       |      |                |                         |                                   |
|---------------------------------|---------|-------|------|----------------|-------------------------|-----------------------------------|
| STATION                         | SURFACE | WIDTH | TYPE | PIPE LNG. SIZE | AREA (yd <sup>2</sup> ) | CEM. CONC. ENT (yd <sup>2</sup> ) |
| 315+85                          | TBB     | 30'   | COM. | -              | 158                     | 71                                |

| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| LT.                                 | 312+00       | 318+00     | 299                      | 518        | -                                  |

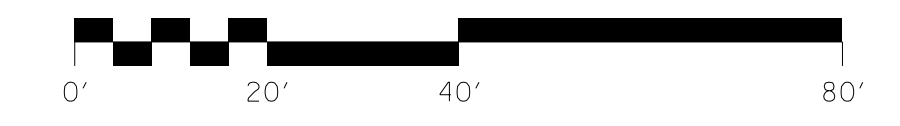
P.I. STA. 323+26.77  
 $\Delta = 29^\circ 48' 52.35" \text{LT}$   
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| DITCH CONSTRUCTION RT. |               |        |                |   |   |            |
|------------------------|---------------|--------|----------------|---|---|------------|
| STA. TO STA.           | SIZE/TYPE     | LENGTH | CHANNEL LINING |   |   |            |
|                        |               |        | TYPE           | D | T | QUANTITY   |
| 312+00 - 314+00        | SPCL. *V* DT. | 200 FT | ECB            | 1 | - | 183 SQ.YD. |
| 314+75 - 315+50        | SPCL. *V* DT. | 75 FT  | ECB            | 1 | - | 69 SQ.YD.  |
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| STANDARD CURB & GUTTER AND SIDEWALK |              |            |                          |            |                                    |
|-------------------------------------|--------------|------------|--------------------------|------------|------------------------------------|
| LT/RT                               | FROM STATION | TO STATION | 5' SW (yd <sup>2</sup> ) | C&G LIN FT | DETECT. WARNING (ft <sup>2</sup> ) |
| RT.                                 | 312+00       | 318+00     | 264                      | 466        | -                                  |

| URBAN ENTRANCE CONSTRUCTION RT. |         |       |      |                |                         |                                   |
|---------------------------------|---------|-------|------|----------------|-------------------------|-----------------------------------|
| STATION                         | SURFACE | WIDTH | TYPE | PIPE LNG. SIZE | AREA (yd <sup>2</sup> ) | CEM. CONC. ENT (yd <sup>2</sup> ) |
| 314+35                          | TBB     | 30'   | COM. | -              | 100                     | 71                                |
| 315+85                          | TBB     | 30'   | COM. | -              | 119                     | 71                                |



MIDTOWN DRIVE  
 PLAN  
 STA. 312+00 TO STA. 318+00

SCALE: 1"=20'

FILE NAME: Y:\NYTC 2014\214-376 SMDRWY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION CABINET\FINAL CONST PLAN SUBMITTAL\2-804.10.R5  
 USER: swagner  
 DATE PLOTTED: November 5, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443

MicroStation v8.11.7.443  
 E-SHEET NAME:  
 USER: swagover  
 DATE PLOTTED: October 29, 2015  
 FILE NAME: Y:\KYTE 2014\214-376 SWRDWY HOPKINS CO.2-804.11\DELIVERABLES\SENT TO TRANSPORTATION CABINET\RW REVISIONS\2-804.10 ROW REV #11215

|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R18       |

REVISED PLAN DATE: NOVEMBER 23, 2015

THE CONTROL OF ACCESS ON THIS PROJECT SHALL BE BY PERMIT

## RIGHT OF WAY SUMMARY

| PARCEL NO. | OWNER(S)              | TOTAL AREA OF TRACT |         | PERMANENT R/W ACQUIRED |         | EASEMENTS |           | AREA SEVERED |         |       |         | EXCESS PURCHASED |         | PORTION REMAINING |         | SEWER SYSTEM TYPE | SEWER SYSTEM AFFECTED BY PROJECT |    | BUILDINGS ACQUIRED NUMBER |   |   |               | SOURCE OF TITLE               | REMARKS* |
|------------|-----------------------|---------------------|---------|------------------------|---------|-----------|-----------|--------------|---------|-------|---------|------------------|---------|-------------------|---------|-------------------|----------------------------------|----|---------------------------|---|---|---------------|-------------------------------|----------|
|            |                       | ACRES               | SQ. FT. | ACRES                  | SQ. FT. | PERMANENT | TEMPORARY | LEFT         |         | RIGHT |         | ACRES            | SQ. FT. | ACRES             | SQ. FT. |                   | YES                              | NO | C                         | R | F | S             |                               |          |
|            |                       |                     |         |                        |         |           |           | SO. FT.      | SO. FT. | ACRES | SO. FT. |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
| 1          | MID TOWN COMMONS, LLC | 208                 |         | 12.338                 |         | 66895     | 197697    |              |         |       |         |                  |         | 195.662           |         |                   |                                  |    |                           |   |   | DB 646 PG 722 | PVA MAP M-42-1-1, M-42-1-4A-3 |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   | DB 665 PG 207 |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |               |                               |          |

NOTE: PERMANENT R/W ACQUIRED + AREA SEVERED = TOTAL AREA OF TRACT.

- TYPE SEWER SYSTEM
1. PRIVATE - INDIVIDUAL
  2. PRIVATE - MULTI PARTY
  3. PUBLIC
  4. NONE
  5. NOT APPLICABLE

- BUILDINGS ACQUIRED CODE
- C - COMMERCIAL
  - R - RESIDENTIAL
  - F - FARM
  - S - STORAGE

\*INCLUDES HAZARDOUS WASTE (UST - UNDERGROUND STORAGE TANKS)

**RIGHT OF WAY SUMMARY SHEET**

THE CONTROL OF ACCESS ON THIS PROJECT SHALL BE BY PERMIT

### RIGHT OF WAY SUMMARY

12.338

| PARCEL NO. | OWNER(S)              | TOTAL AREA OF TRACT |         | PERMANENT R/W ACQUIRED |         | EASEMENTS |           | AREA SEVERED |         |       |         | EXCESS PURCHASED |         | PORTION REMAINING |         | SEWER SYSTEM TYPE | SEWER SYSTEM AFFECTED BY PROJECT |    | BUILDINGS ACQUIRED NUMBER |   |   |   | SOURCE OF TITLE | REMARKS*      |                               |  |
|------------|-----------------------|---------------------|---------|------------------------|---------|-----------|-----------|--------------|---------|-------|---------|------------------|---------|-------------------|---------|-------------------|----------------------------------|----|---------------------------|---|---|---|-----------------|---------------|-------------------------------|--|
|            |                       | ACRES               | SQ. FT. | ACRES                  | SQ. FT. | PERMANENT | TEMPORARY | LEFT         |         | RIGHT |         | ACRES            | SQ. FT. | ACRES             | SQ. FT. |                   | YES                              | NO | C                         | R | F | S |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           | SQ. FT.      | SQ. FT. | ACRES | SQ. FT. |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
| 1          | MID TOWN COMMONS, LLC | 208                 |         | 12.475                 |         | 66895     | 194430    |              |         |       |         |                  |         | 195.525           |         |                   |                                  |    |                           |   |   |   |                 | DB 646 PG 722 | PVA MAP M-42-1-1, M-42-1-4A-3 |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 | DB 665 PG 207 |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |
|            |                       |                     |         |                        |         |           |           |              |         |       |         |                  |         |                   |         |                   |                                  |    |                           |   |   |   |                 |               |                               |  |

NOTE: PERMANENT R/W ACQUIRED + AREA SEVERED = TOTAL AREA OF TRACT.

TYPE SEWER SYSTEM  
 1. PRIVATE - INDIVIDUAL  
 2. PRIVATE - MULTI PARTY  
 3. PUBLIC  
 4. NONE  
 5. NOT APPLICABLE

BUILDINGS ACQUIRED CODE  
 C - COMMERCIAL  
 R - RESIDENTIAL  
 F - FARM  
 S - STORAGE

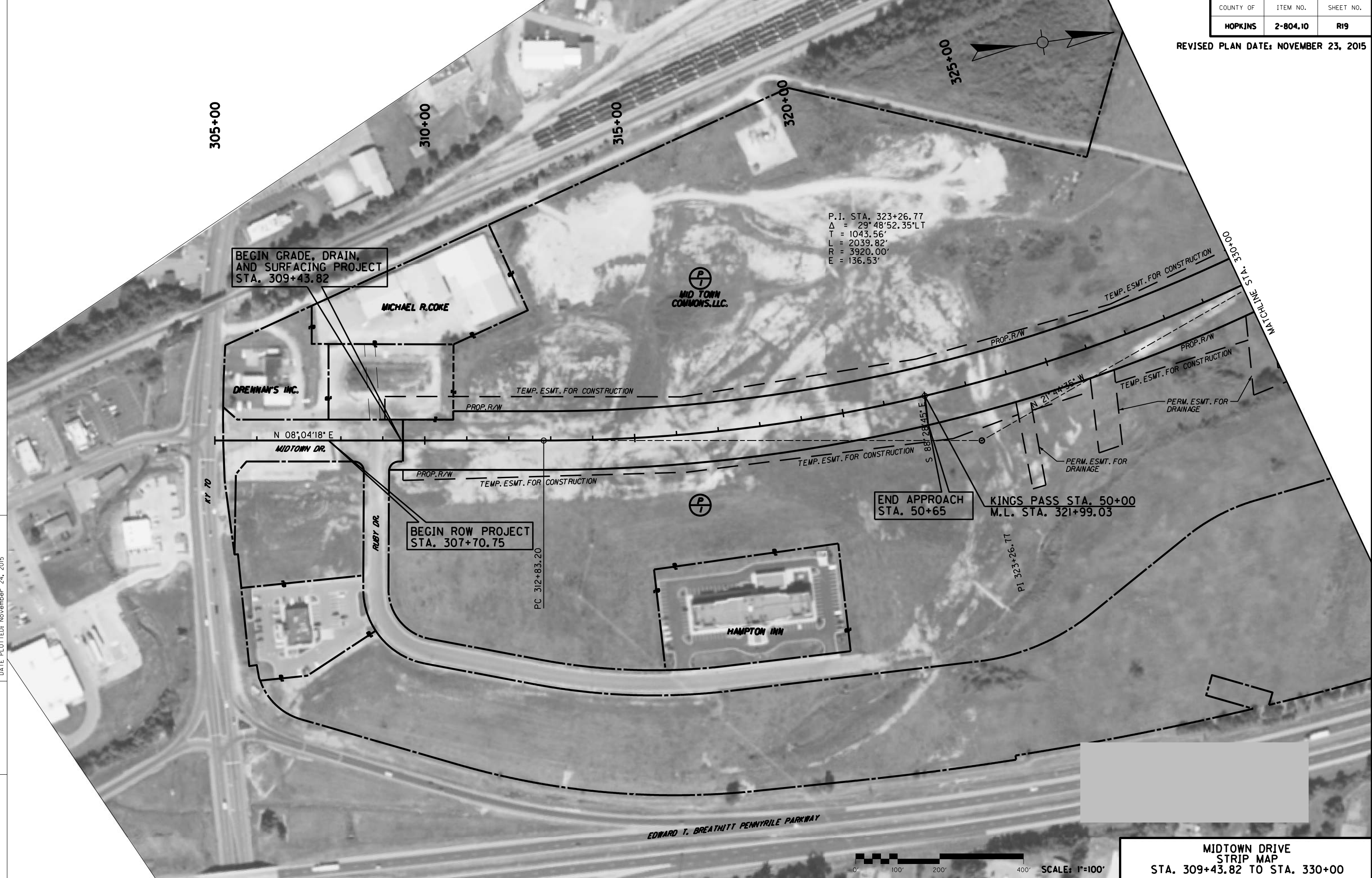
\*INCLUDES HAZARDOUS WASTE (UST - UNDERGROUND STORAGE TANKS)

FILE NAME: Y:\NYTC 2014\214-376 SMDWY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION CABINET\FINAL CONST PLAN SUBMITTAL 2-804.10  
 USER: swaober  
 DATE PLOTTED: November 5, 2015  
 E-SHEET NAME:  
 MicroStation v8.1i.7.443

MicroStation v8.11.7.443 E-SHEET NAME: USER: sweover DATE PLOTTED: November 24, 2015 FILE NAME: Y:\KYTC\2014\214-376 SMDWY HOPKINS CO.2-804.H\DELIVERABLES\SENT TO TRANSPORTATION CABINET\RW REVISIONS\2-804.10 ROW REV #123

|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R19       |

REVISED PLAN DATE: NOVEMBER 23, 2015



BEGIN GRADE, DRAIN, AND SURFACING PROJECT STA. 309+43.82

BEGIN ROW PROJECT STA. 307+70.75

END APPROACH STA. 50+65

KINGS PASS STA. 50+00 M.L. STA. 321+99.03

P.I. STA. 323+26.77  
Δ = 29° 48' 52.35" LT  
T = 1043.56'  
L = 2039.82'  
R = 3920.00'  
E = 136.53'

MIDTOWN DRIVE STRIP MAP STA. 309+43.82 TO STA. 330+00



EDWARD T. BREATHTT PENNSYLVANIA PARKWAY



|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R19       |



BEGIN GRADE, DRAIN,  
AND SURFACING PROJECT  
STA. 309+43.82

ROW limits change

BEGIN ROW PROJECT  
STA. 307+70.75

END APPROACH  
STA. 50+65

KINGS PASS STA. 50+00  
M.L. STA. 321+99.03

P.I. STA. 323+26.77  
Δ = 29° 48' 52.35" LT  
T = 1043.56'  
L = 2039.82'  
R = 3920.00'  
E = 136.53'

FILE NAME: Y:\KYTC\2014\214-376 SRD\DWY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION\CABINET\FINAL CONST PLAN SUBMITTAL 2-804.10  
 USER: swaover  
 DATE PLOTTED: November 5, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443

MIDTOWN DRIVE  
STRIP MAP  
STA. 309+43.82 TO STA. 330+00

SCALE: 1"=100'



### BASIS OF ELEVATIONS

Elevations were derived from Digital Levels and GPS methods and are based off of NGS Control Monument KYTB RM 1. Geoid model used was Geoid 12a.

### PROJECT COORDINATES

Coordinates for horizontal control were obtained from GPS methods and adjusted to the National NAD83/FBN System.

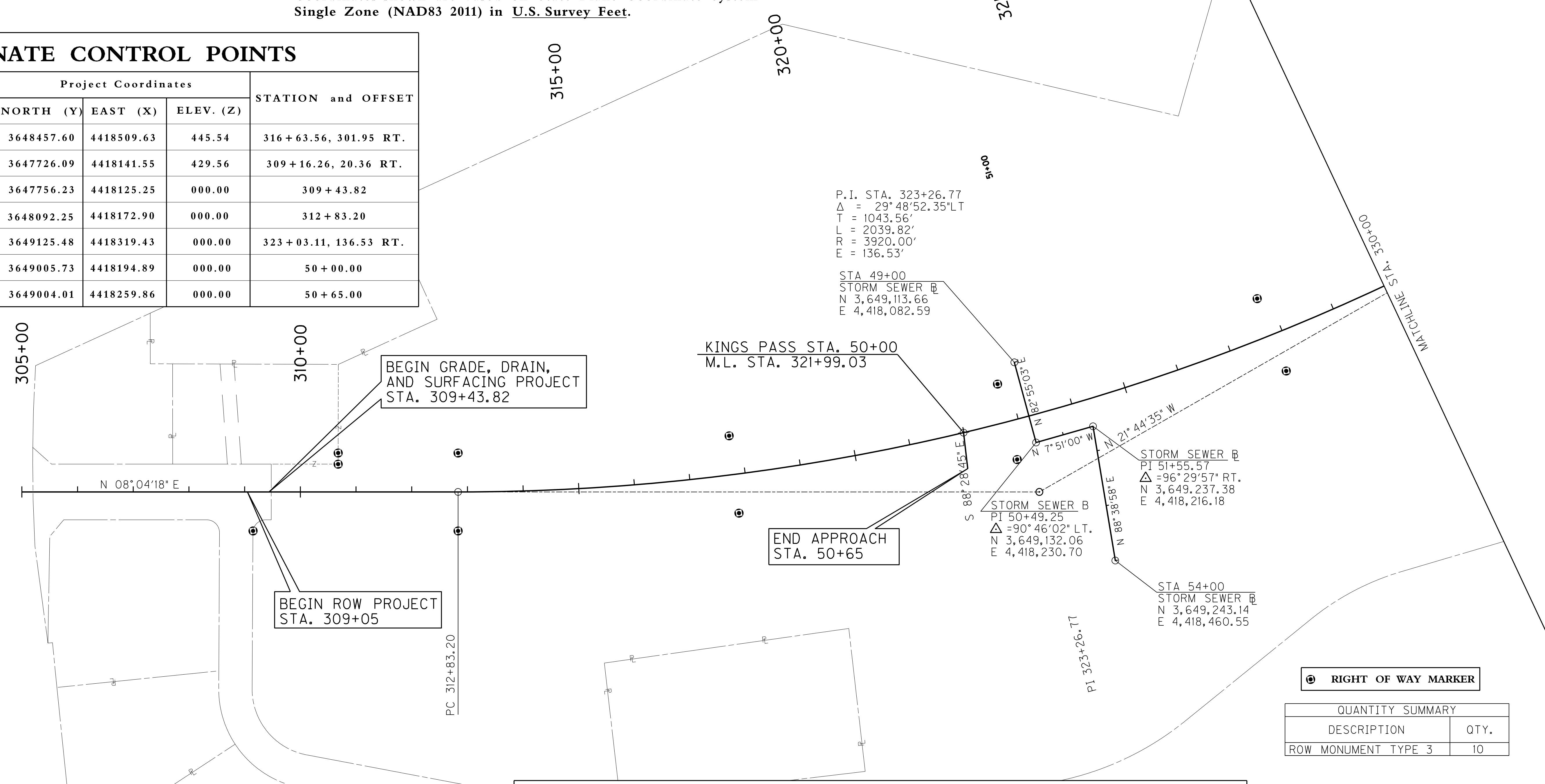
Coordinates shown are based on State Plane Coordinate System Single Zone (NAD83 2011) in U.S. Survey Feet.

|           |          |           |
|-----------|----------|-----------|
| COUNTY OF | ITEM NO. | SHEET NO. |
| HOPKINS   | 2-804.10 | R37       |

REVISED PLAN DATE: NOVEMBER 23, 2015

### COORDINATE CONTROL POINTS

| POINT | DESCRIPTION    | Project Coordinates |            |           | STATION and OFFSET      |
|-------|----------------|---------------------|------------|-----------|-------------------------|
|       |                | NORTH (Y)           | EAST (X)   | ELEV. (Z) |                         |
| CP-1  | IRON PIN W/CAP | 3648457.60          | 4418509.63 | 445.54    | 316 + 63.56, 301.95 RT. |
| CP-2  | IRON PIN W/CAP | 3647726.09          | 4418141.55 | 429.56    | 309 + 16.26, 20.36 RT.  |
| POB   | MAINLINE       | 3647756.23          | 4418125.25 | 000.00    | 309 + 43.82             |
| PC    | MAINLINE       | 3648092.25          | 4418172.90 | 000.00    | 312 + 83.20             |
| PI    | PI 323 + 26.77 | 3649125.48          | 4418319.43 | 000.00    | 323 + 03.11, 136.53 RT. |
| POB   | KINGS PASS     | 3649005.73          | 4418194.89 | 000.00    | 50 + 00.00              |
| POE   | KINGS PASS     | 3649004.01          | 4418259.86 | 000.00    | 50 + 65.00              |



⊙ RIGHT OF WAY MARKER

| QUANTITY SUMMARY    |      |
|---------------------|------|
| DESCRIPTION         | QTY. |
| ROW MONUMENT TYPE 3 | 10   |

### RIGHT OF WAY MONUMENT POINTS

| ALIGNMENT     | STATION     | OFFSET     | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|---------------|-------------|------------|------|--------------|-------------------------|------------|
|               |             |            |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE | 309 + 15.06 | 70.0' RT   | 3    | ROW MONUMENT | 3647717.93              | 4418190.52 |
| MIDTOWN DRIVE | 310 + 67.84 | 49.94' LT. | 3    | ROW MONUMENT | 3647886.04              | 4418093.22 |
| MIDTOWN DRIVE | 310 + 67.89 | 70.0' LT.  | 3    | ROW MONUMENT | 3647888.90              | 4418073.36 |
| MIDTOWN DRIVE | 312 + 83.20 | 70.0' LT.  | 3    | ROW MONUMENT | 3648102.08              | 4418103.60 |
| MIDTOWN DRIVE | 312 + 83.20 | 70.0' RT.  | 3    | ROW MONUMENT | 3648082.42              | 4418242.21 |
| MIDTOWN DRIVE | 317 + 80.00 | 70.0' LT.  | 3    | ROW MONUMENT | 3648588.22              | 4418141.35 |

### RIGHT OF WAY MONUMENT POINTS

| ALIGNMENT     | STATION  | OFFSET    | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|---------------|----------|-----------|------|--------------|-------------------------|------------|
|               |          |           |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE | 317 + 80 | 70.0' RT  | 3    | ROW MONUMENT | 3648586.24              | 4418281.34 |
| MIDTOWN DRIVE | 322 + 80 | 70.0' LT. | 3    | ROW MONUMENT | 3649078.35              | 4418117.01 |
| MIDTOWN DRIVE | 322 + 80 | 70.0' RT  | 3    | ROW MONUMENT | 3649094.19              | 4418256.11 |
| MIDTOWN DRIVE | 327 + 80 | 70.0' LT. | 3    | ROW MONUMENT | 3649561.40              | 4418030.51 |
| MIDTOWN DRIVE | 327 + 80 | 70.0' RT. | 3    | ROW MONUMENT | 3649594.81              | 4418166.46 |



MIDTOWN DRIVE  
COORDINATE CONTROL SHEET  
STA. 309+43.82 TO STA. 330+00

FILE NAME: G:\PWORK\CHARLOTTE\COTTON\1057613\COORD. CNTL 1.DGN  
 USER: charlotte.cotton  
 DATE PLOTTED: November 24, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443

### BASIS OF ELEVATIONS

Elevations were derived from Digital Levels and GPS methods and are based off of NGS Control Monument KYTB RM 1. Geoid model used was Geoid 12a.

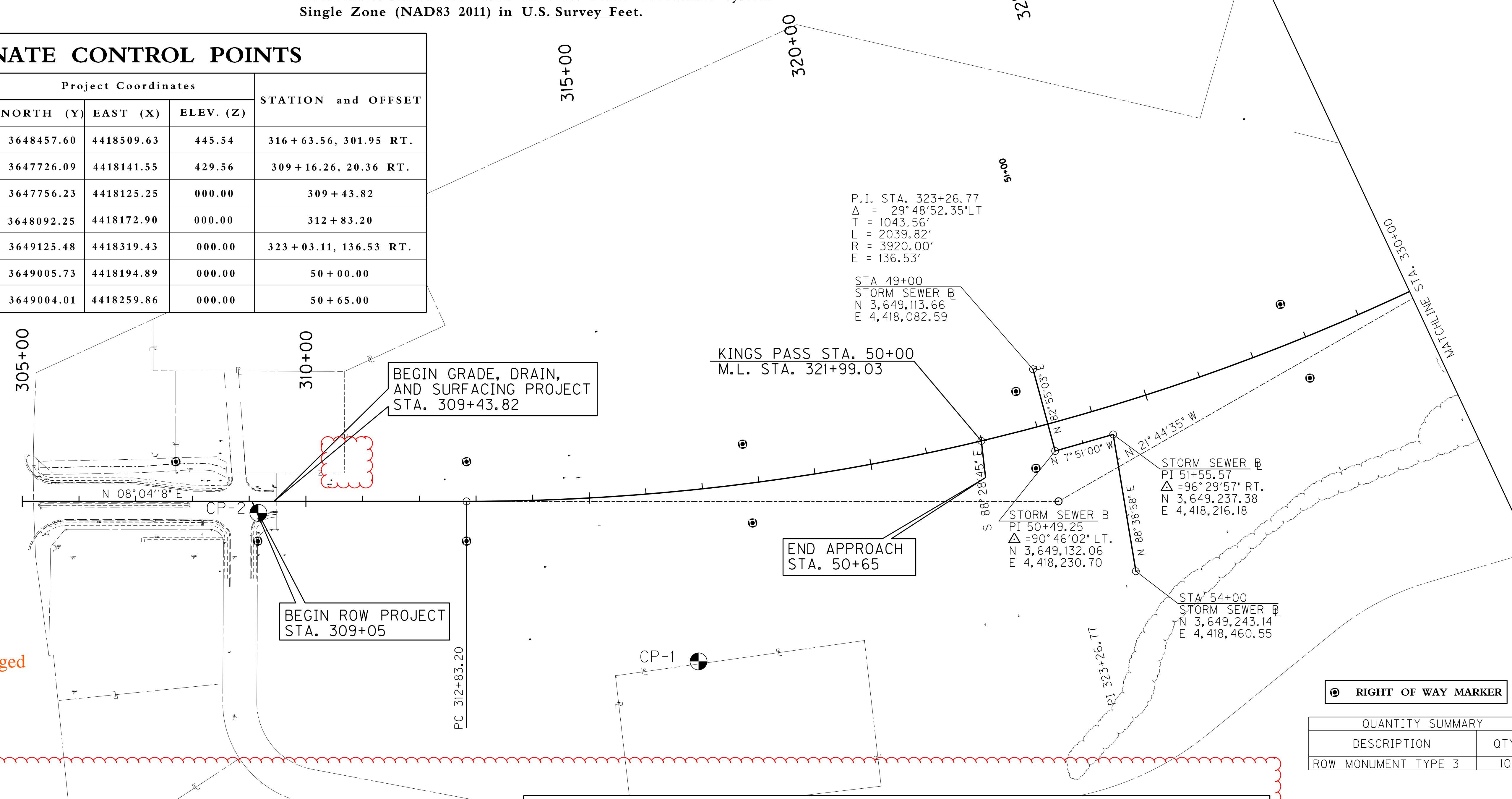
### PROJECT COORDINATES

Coordinates for horizontal control were obtained from GPS methods and adjusted to the National NAD83/FBN System.

Coordinates shown are based on State Plane Coordinate System Single Zone (NAD83 2011) in U.S. Survey Feet.

### COORDINATE CONTROL POINTS

| POINT | DESCRIPTION    | Project Coordinates |            |           | STATION and OFFSET      |
|-------|----------------|---------------------|------------|-----------|-------------------------|
|       |                | NORTH (Y)           | EAST (X)   | ELEV. (Z) |                         |
| CP-1  | IRON PIN W/CAP | 3648457.60          | 4418509.63 | 445.54    | 316 + 63.56, 301.95 RT. |
| CP-2  | IRON PIN W/CAP | 3647726.09          | 4418141.55 | 429.56    | 309 + 16.26, 20.36 RT.  |
| POB   | MAINLINE       | 3647756.23          | 4418125.25 | 000.00    | 309 + 43.82             |
| PC    | MAINLINE       | 3648092.25          | 4418172.90 | 000.00    | 312 + 83.20             |
| PI    | PI 323 + 26.77 | 3649125.48          | 4418319.43 | 000.00    | 323 + 03.11, 136.53 RT. |
| POB   | KINGS PASS     | 3649005.73          | 4418194.89 | 000.00    | 50 + 00.00              |
| POE   | KINGS PASS     | 3649004.01          | 4418259.86 | 000.00    | 50 + 65.00              |



Monument Information Changed

RIGHT OF WAY MARKER

| QUANTITY SUMMARY    |      |
|---------------------|------|
| DESCRIPTION         | QTY. |
| ROW MONUMENT TYPE 3 | 10   |

### RIGHT OF WAY MONUMENT POINTS

| ALIGNMENT     | STATION     | OFFSET    | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|---------------|-------------|-----------|------|--------------|-------------------------|------------|
|               |             |           |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE | 309 + 47.86 | 49.94' LT | 3    | ROW MONUMENT | 3647767.24              | 4418076.37 |
| MIDTOWN DRIVE | 309 + 47.86 | 50.0' RT. | 3    | ROW MONUMENT | 3647753.20              | 4418175.38 |
| MIDTOWN DRIVE | 312 + 83.20 | 50.0' LT. | 3    | ROW MONUMENT | 3648099.27              | 4418123.40 |
| MIDTOWN DRIVE | 312 + 83.20 | 50.0' RT. | 3    | ROW MONUMENT | 3648085.23              | 4418222.41 |
| MIDTOWN DRIVE | 317 + 80    | 50.0' LT. | 3    | ROW MONUMENT | 3648587.93              | 4418161.35 |

### RIGHT OF WAY MONUMENT POINTS

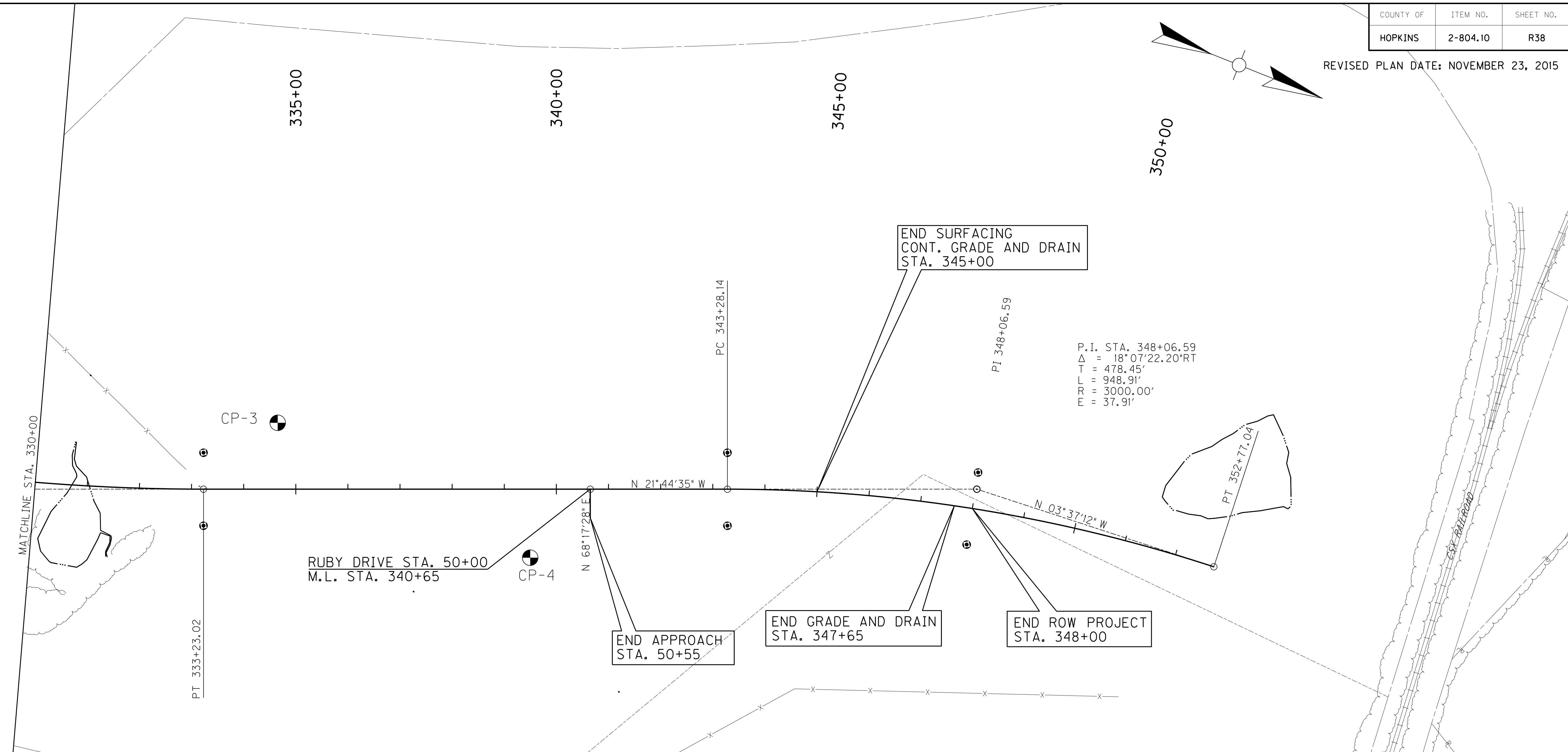
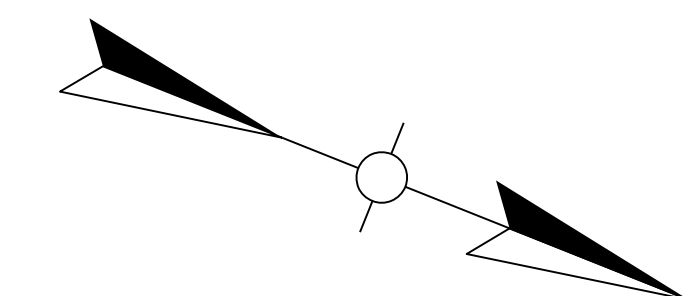
| ALIGNMENT     | STATION  | OFFSET    | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|---------------|----------|-----------|------|--------------|-------------------------|------------|
|               |          |           |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE | 317 + 80 | 50.0' RT  | 3    | ROW MONUMENT | 3648586.52              | 4418261.34 |
| MIDTOWN DRIVE | 322 + 80 | 50.0' LT. | 3    | ROW MONUMENT | 3649080.61              | 4418136.88 |
| MIDTOWN DRIVE | 322 + 80 | 50.0' RT  | 3    | ROW MONUMENT | 3649091.93              | 4418236.23 |
| MIDTOWN DRIVE | 327 + 80 | 50.0' LT. | 3    | ROW MONUMENT | 3649566.18              | 4418049.93 |
| MIDTOWN DRIVE | 327 + 80 | 50.0' RT. | 3    | ROW MONUMENT | 3649590.04              | 4418147.04 |



MIDTOWN DRIVE  
COORDINATE CONTROL SHEET  
STA. 309+43.82 TO STA. 330+00

FILE NAME: Y:\KYTC\2014\214-376 SMDRY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION CABINET\FINAL CONST PLAN SUBMITTAL\2-804.10\...  
 USER: swaover  
 DATE PLOTTED: January 1, 0001  
 E-SHEET NAME: E-SHEET NAME:  
 MicroStation v8.11.7.443





**COORDINATE CONTROL POINTS**

| POINT | DESCRIPTION        | Project Coordinates |            |           | STATION and OFFSET     |
|-------|--------------------|---------------------|------------|-----------|------------------------|
|       |                    | NORTH (Y)           | EAST (X)   | ELEV. (Z) |                        |
| CP-3  | IRON PIN W/CAP     | 3650179.85          | 4417761.92 | 455.14    | 334 + 65.34, 127.26 LT |
| CP-4  | IRON PIN W/CAP     | 3650725.63          | 4417822.74 | 460.17    | 339 + 49.76, 131.41 RT |
| PT    | MAINLINE           | 3650094.80          | 4417932.85 | 000.00    | 333 + 23.02            |
| PC    | MAINLINE           | 3651028.40          | 4417560.51 | 000.00    | 343 + 28.14            |
| PI    | PI 348 + 06.59     | 3651472.81          | 4417383.27 | 000.00    | 348 + 02.59, 37.91 LT. |
| PT    | MAINLINE           | 3651950.31          | 4417353.06 | 000.00    | 352 + 77.04            |
| POE   | MAINLINE (SURFACE) | 3651189.78          | 4417501.45 | 000.00    | 345 + 00.00            |
| POE   | MAINLINE (G & D)   | 3651444.52          | 4417428.74 | 000.00    | 347 + 65.00            |
| POB   | RUBY DRIVE         | 3650783.99          | 4417657.99 | 000.00    | 50 + 00.00             |
| POE   | RUBY DRIVE         | 3650804.33          | 4417709.09 | 000.00    | 50 + 55.00             |

**RIGHT OF WAY MONUMENT POINTS**

| ALIGNMENT     | STATION     | OFFSET    | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|---------------|-------------|-----------|------|--------------|-------------------------|------------|
|               |             |           |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE | 333 + 23.02 | 70.0' LT. | 3    | ROW MONUMENT | 3650068.87              | 4417867.83 |
| MIDTOWN DRIVE | 333 + 23.02 | 70.0' RT. | 3    | ROW MONUMENT | 3650120.73              | 4417997.87 |
| MIDTOWN DRIVE | 343 + 28.14 | 70.0' LT. | 3    | ROW MONUMENT | 3651002.47              | 4417495.49 |
| MIDTOWN DRIVE | 343 + 28.14 | 70.0' RT. | 3    | ROW MONUMENT | 3651054.33              | 4417625.53 |
| MIDTOWN DRIVE | 348 + 00.00 | 70.0' LT. | 3    | ROW MONUMENT | 3651463.18              | 4417352.55 |
| MIDTOWN DRIVE | 348 + 00.00 | 70.0' RT. | 3    | ROW MONUMENT | 3651494.04              | 4417489.11 |

| QUANTITY SUMMARY    |      |
|---------------------|------|
| DESCRIPTION         | QTY. |
| ROW MONUMENT TYPE 3 | 6    |

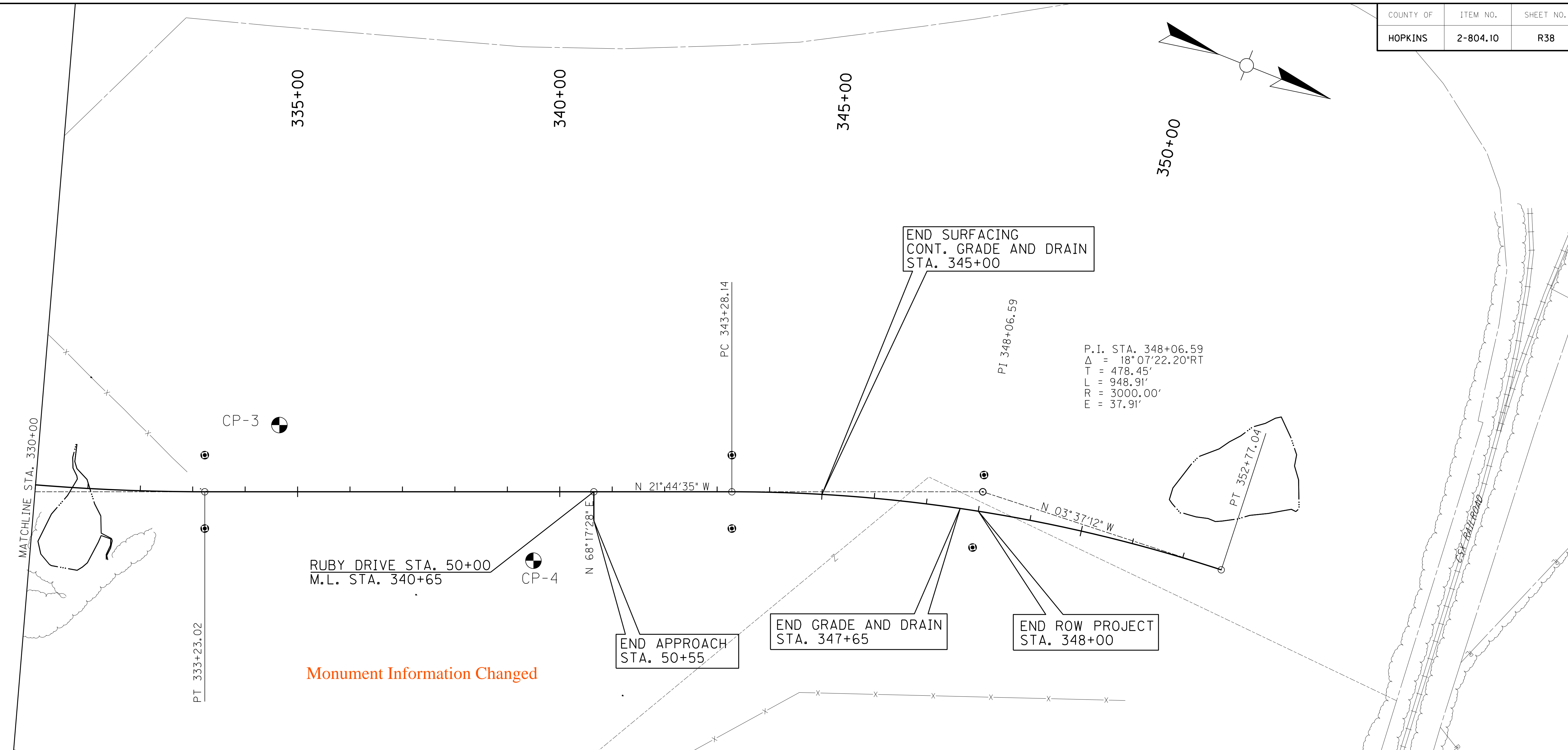
RIGHT OF WAY MARKER



MIDTOWN DRIVE  
COORDINATE CONTROL SHEET  
STA. 330+00 TO STA. 348+00

FILE NAME: G:\PWORK\CHARLOTTE\COTTON\DI057613\COORD CNTL 2.DGN  
 USER: charlotte.cotton  
 DATE PLOTTED: November 24, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443

FILE NAME: Y:\KYTC\2014\214-376 SMDRY HOPKINS CO.2-804.10\DELIVERABLES\SENT TO TRANSPORTATION\CABINET\_FINAL CONST PLAN SUBMITTAL\2-804.10\...  
 USER: swagover  
 DATE PLOTTED: November 7, 2015  
 E-SHEET NAME:  
 MicroStation v8.11.7.443



Monument Information Changed

| COORDINATE CONTROL POINTS |                    |                     |            |           |                        |
|---------------------------|--------------------|---------------------|------------|-----------|------------------------|
| POINT                     | DESCRIPTION        | Project Coordinates |            |           | STATION and OFFSET     |
|                           |                    | NORTH (Y)           | EAST (X)   | ELEV. (Z) |                        |
| CP-3                      | IRON PIN W/CAP     | 3650179.85          | 4417761.92 | 455.14    | 334 + 65.34, 127.26 LT |
| CP-4                      | IRON PIN W/CAP     | 3650725.63          | 4417822.74 | 460.17    | 339 + 49.76, 131.41 RT |
| PT                        | MAINLINE           | 3650094.80          | 4417932.85 | 000.00    | 333 + 23.02            |
| PC                        | MAINLINE           | 3651028.40          | 4417560.51 | 000.00    | 343 + 28.14            |
| PI                        | PI 348 + 06.59     | 3651472.81          | 4417383.27 | 000.00    | 348 + 02.59, 37.91 LT. |
| PT                        | MAINLINE           | 3651950.31          | 4417353.06 | 000.00    | 352 + 77.04            |
| POE                       | MAINLINE (SURFACE) | 3651189.78          | 4417501.45 | 000.00    | 345 + 00.00            |
| POE                       | MAINLINE (G & D)   | 3651444.52          | 4417428.74 | 000.00    | 347 + 65.00            |
| POB                       | RUBY DRIVE         | 3650783.99          | 4417657.99 | 000.00    | 50 + 00.00             |
| POE                       | RUBY DRIVE         | 3650804.33          | 4417709.09 | 000.00    | 50 + 55.00             |

| RIGHT OF WAY MONUMENT POINTS |             |           |      |              |                         |            |
|------------------------------|-------------|-----------|------|--------------|-------------------------|------------|
| ALIGNMENT                    | STATION     | OFFSET    | TYPE | DESCRIPTION  | State Plane Coordinates |            |
|                              |             |           |      |              | NORTH (Y)               | EAST (X)   |
| MIDTOWN DRIVE                | 333 + 23.02 | 50.0' LT. | 3    | ROW MONUMENT | 3650076.27              | 4417886.41 |
| MIDTOWN DRIVE                | 333 + 23.02 | 50.0' RT. | 3    | ROW MONUMENT | 3650113.32              | 4417979.29 |
| MIDTOWN DRIVE                | 343 + 28.14 | 50.0' LT. | 3    | ROW MONUMENT | 3651009.88              | 4417514.07 |
| MIDTOWN DRIVE                | 343 + 28.14 | 50.0' RT. | 3    | ROW MONUMENT | 3651046.92              | 4417606.96 |
| MIDTOWN DRIVE                | 348 + 00.00 | 50.0' LT. | 3    | ROW MONUMENT | 3651467.59              | 4417372.06 |
| MIDTOWN DRIVE                | 348 + 00.00 | 50.0' RT. | 3    | ROW MONUMENT | 3651489.63              | 4417469.60 |

| QUANTITY SUMMARY    |      |
|---------------------|------|
| DESCRIPTION         | QTY. |
| ROW MONUMENT TYPE 3 | 6    |

RIGHT OF WAY MARKER



MIDTOWN DRIVE  
 COORDINATE CONTROL SHEET  
 STA. 330+00 TO STA. 348+00